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Sustainable Landscapes and Economy

On the inestimable natural and human value of the landscape

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Abstract

For many centuries Kadıköy, served as a country side to the Ottoman Empire and as a connection of Europe to Asia. Especially during the Turkish Republic, the Haydarpaşa train station was seen as an important opportunity. Its connecting role between ruralty and city became a national symbol in the public. Kadıköy which is settled on 6 hills of Istanbul includes many urban varieties such as seafront, pier, historic market places and the Moda neighborhood.

Train stations are generally accepted as urban gates of cities which have an important visual perception to visitors. In Istanbul and Turkey, Haydarpaşais imprinted to peoples mind as a landmark. Although, its architectural quality, the location of the building and landscape is also an important factor in this visual perception. Beside its reputation in Turkish “Yeşilçam” movie sector and Istanbul introductions,

preserving the building as it is and locating tracks in redesigned landscape is aimed to hand down this beauty to the next generations.

Kadiköy seafront can be considered as one of the most important shores in the city. Removing bus and minibus stops, pedestrianising the seafront and piazza, rehabilitating the sea transportation and pier, creating connection both by rail road or pedestrian way and completing path with creating a land fill park will build continuous circulation axe, visual scenery and will ease public connection.

Haydarpaşa train station and Moda Street are proposed to become a forefront design in the area and became to be a part of the seafront circulation. Behind the site, parallel to sea, Historic Mühürdar market and other neighborhoods will support and feed pedestrian flow to the site and will help to create a complete circulation to Haydarpaşa station. This proposal will remind the local people and give tourists the historical essence of the neighborhood.

A northeast and southwest directional pedestrian bridge design is proposed to be a salvation to traffic problems and it will also connect to Üsküdar district next to Kadiköy neighborhood. To emphasize this important city axe it is traced to the sea and created a giant water curtain in the sea. By this proposal in Kadiköy seafront design there will also be continuous action and clean air while people are shopping and experiencing Turkish traditions.

Keywords: Urban Design, Landscape Architecture, Planning, Kadiköy, Haydarpaşa

Introduction

Kadiköy, which has been the most important summer resort for centuries at the Ottoman Empire and links Europe to Anatolia, is the most important distribution center of the republic period and the first standpoint for the immigrants coming to İstanbul, features with the historical Haydarpaşa station. Station buildings are the entrance doors for the cities where they are located. For the ones who will visit the city, they are the perception points just like Haydarpaşa station which is the first point where many people commit İstanbul to memory. The impacts of location and landscaping of building are important as much as architectural qualifications of station buildings in this perception. In addition to being subject of many Turkish cinema films, the station structure which is one of the most important actors of introductory films of İstanbul, are also transferred to next generations by emphasising it through protecting the rails within new and natural landscaping.

Project Approach

Haydarpaşa station is not only an architectural sample which stays in the memory of the city, but also a symbol of İstanbul. Kadiköy, which is located on six hills of İstanbul, has a cultural variety, beach, station, old bazaar, Moda and many civic privileges. However, various functions and usages which are shaped in time through the Kadiköy quay (marine transportation, bus stations, minibus stations, demolished meat and fish authority, etc.) affected the spatial variety negatively and Kadiköy quay did not reach the desired reactive liveliness (Figure 01). The places, except for the ones used for recreation, which are located through the quay and beach penetrated into the pattern of neighborhoods and this prevented the

communication between neighborhoods and districts laid through the quay because of deepened spatial disintegration. This spatial differentiation affected the back side in time and minimised the utilisation to transportation through the quay in time and utilisation of places with the aim of recreation by public users are slipped from seaside to interior sides around Moda and its periphery. (Figure 2). Except for the square which becomes active many times in a year in line with the festivals and activities, utilisation of the seaside is transportation-based. As a result, despite of being active almost all the times in a day, Kadiköy quay failed in turning this dynamism into public utilisation.



Figure 01



Figure 02

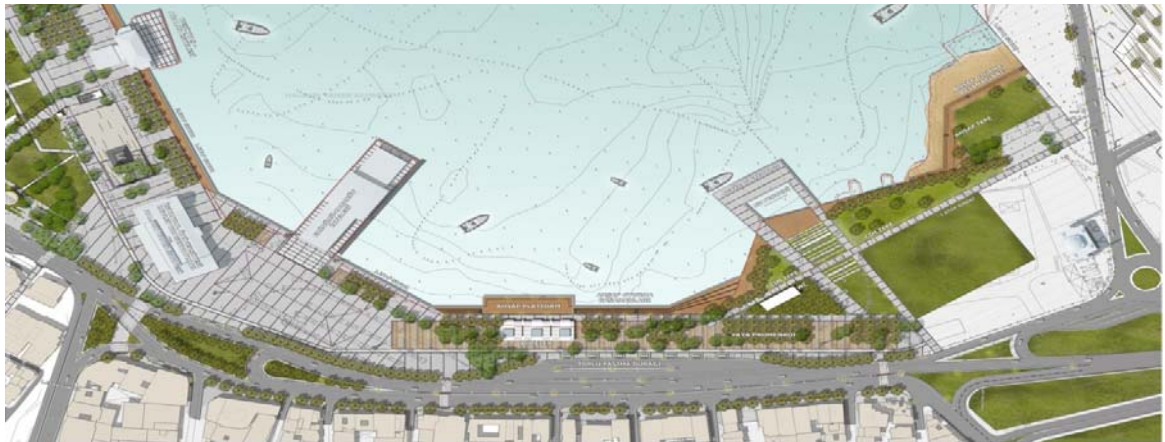


Figure 03

Kadiköy quay is not the only one of İstanbul, but it may be the most important one. It is the speech of city with sea. By removing the bus and minibüs stations, pedestrianising the seaside and by creating a new city square, enhancing the marine transportation and seaport, by connecting Kadiköy–Haydarpaşa not only via pedestrian axis and railway and by maintaining it via fill area, shore line park to Moda; visual continuance of coasts is ensured and also accessibility is ensured, too (Figure 03). The spatial (locational) potential which emerged as a result of short time stops of bus and minibüs stations and the ring system gives the change to create a pedestrian promenade which is parallel to axis of street and which has green structure. This promenade is enriched with buffets, wooden platforms which are located along the coast side that is enriched with sitting areas under the trees and enhanced with wooden platforms and an observation terrace. The observation platforms and wooden platforms continue through the coast that reaches to Haydarpaşa Station with various re-creative activities and physically links the coastal side of Kadiköy Quay to station building which is one of the symbols of İstanbul (Figure 04 - 05).



Figure 04

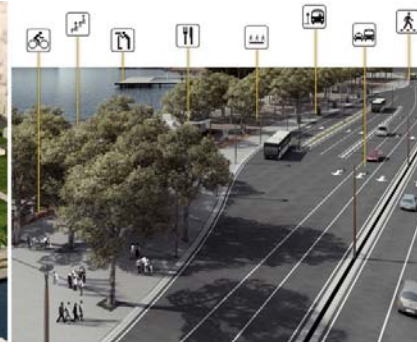


Figure 05

The route located in south–eastern direction and which includes the square, ports and Haldun Taner Stage and reaches to İnciburnu Pilot Station via fast ferry dock is arranged substantially and vegetal arrangements, great trees and re-arranged pedestrian axis are coupled the dense city pattern with the seaside which lays through the quay and with the silhouette of İstanbul.

The exit points of metro stations that are located around this axis are evaluated together with the renewed spatial concept and public character of the location and came into prominence by optimising the user circulation. The bus stations and the access roads which are located on the route are displaced and Kadiköy Bazaar and coast are approached to each other spatially, their integrity is enhanced (Figure 06).



Figure 06

The coastal axis that reaches to the south from İnci Burnu and continues to Moda Sea Club is re-arranged and the relation between Kadiköy Quay and Moda and is perpetuated and a continuing coastal line is ensured. The functions which are found through the axis and became inactive are enhanced via integrated designs. The filling structure that is found in the coast is especially shaped and it is aimed to give acceleration to social mobility by suggesting the roller section and restorations. Suggested observation terraces are varied via grasses and picnic sites as seasonal passive recreation and via sports areas and the game zones which attract the attention of different age groups. A design which can be utilised actively by all the users in the yacht club which is suggested to be created on the side where the land and Moda Sea Club are connected thanks to the parking areas which are appropriate structurally (Figure 07).



Figure 07

As it can be monitored above, Kadiköy seaside and quay promenade, where various stages and functions are found, is fictionalised with an integrative approach at high scale and it is aimed to spread the public usage to the entire field via suggested space usage and motion fiction in subscale. It is aimed to isolate the public usage dynamism from physical barriers of urban structure and neighborhood pattern with integrity (Figure 08)



Figure 08

Result

As the stages of the project are realised, it is predicted that public utilisation will impact positively and betterments of the public sphere utilisation and transportation which are suggested along the Kadiköy quay will strengthen the relation between coastal line and Rasimpaşa, Yeldeğirmeni and Osmanağa neighborhoods. Reunion of inner neighborhoods, Altıyol and Moda is aimed with isolated Haydarpaşa. By featuring the streets of Kadiköy, Haydarpaşa and Moda which are vertical to the sea, they become a part of coastal layout. It is aimed to reunite the old Mühürdar Bazaar of Kadiköy coast which is planned to be turned into a real cordon with the back streets which are parallel to the sea and with Haydarpaşa. Project İstanbul is an integrity of suggestions which remembers the already known open – air spaces. This attitude will also give acceleration to urban renewal seen in Yeldeğirmeni and meets the urban axis and transportation focuses (Figure 09).

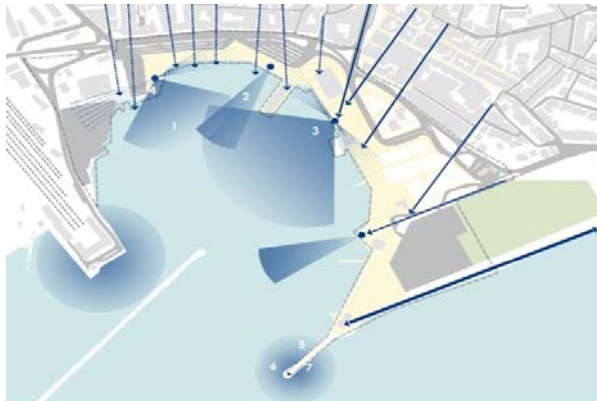


Figure 09

The streets and roads will also become more active and they will meet the public functions and become alive as a natural result of raising spatial quality as a result of design. The contributions to local and urban economy, many samples of which are seen around the world when consecutive results of such projects emerge and qualified applications are realised, shall be seen in time. The pedestrian bridge which is suggested to be built through the quay in north eastern – south western direction will become a solution for traffic congestions and also links the coast to Üsküdar and creates a new urban axis with a great water screen in the sea. It will be possible to live the urban culture and go shopping by walking on the axis where the motions are continuous and where there is no exhaust smoke and this axis shall ensure the integrity and harmonisation of neighborhood pattern, which is eroded and under the renewal, with the coastal line. The biggest advantage in this point is that the street pattern which unites with the quay axis is vertical to the coast and the created vistas complete each other with overlapping functions.

Thanks to the amending in urban silhouette through the quay and thanks to the variation with more green, more public and more active nature, the coast of Kadiköy will reach to the desired character.